

## LONDON BOROUGH OF ENFIELD

### PLANNING COMMITTEE

Date : 19/07/2016

**Report of**  
Assistant Director, Planning,  
Highways & Transportation

**Contact Officer:**  
Andy Higham  
Sharon Davidson  
Mr Kevin Tohill

**Ward:**  
Edmonton Green

Ref: 15/04513/FUL

Category: Full Application

**LOCATION: Monmouth Road Car Park, 35-36 North Square And North Mall, Edmonton Shopping Centre, London**

**PROPOSAL:** Erection of a 9-storey block to provide 77 residential units (comprising 28 x 1-bed and 49 x 2-bed flats) with balconies to front and rear elevations and solar panels to roof involving reduction in parking spaces and the reconfiguration of car park, conversion of vacant first floor commercial space (Class B1) to provide 5 x 3-bed residential units with amenity deck and refurbishment of North Mall retail thoroughfare.

**Applicant Name & Address:**

C/O Agent  
United Kingdom

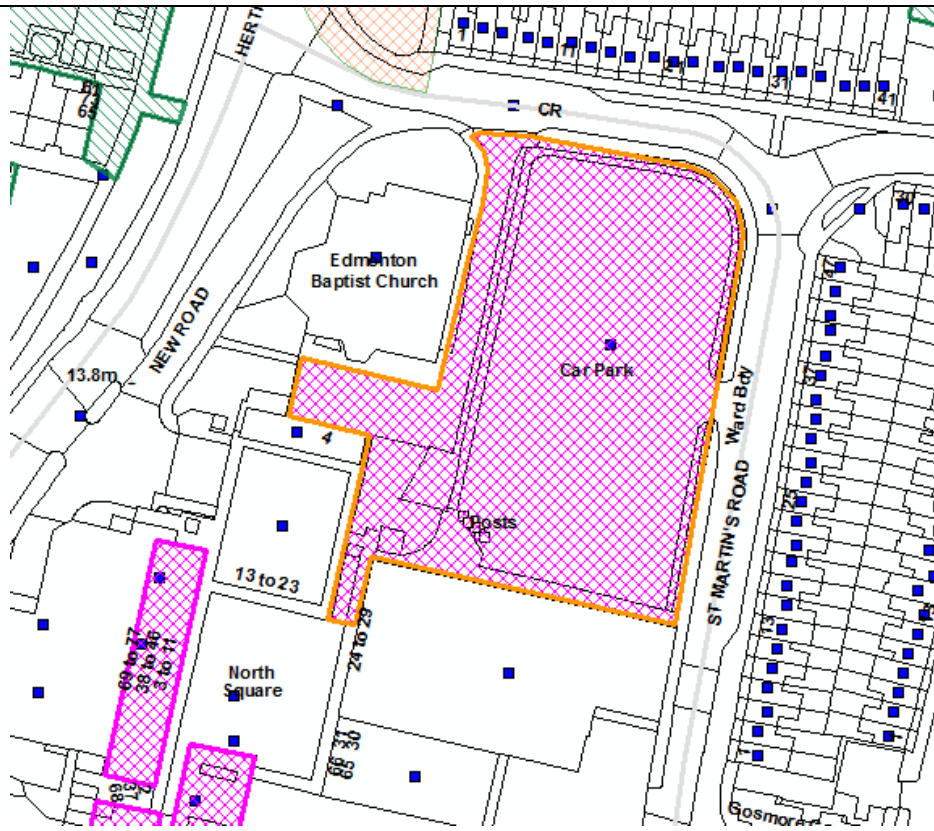
**Agent Name & Address:**

Mrs Claire Wilkinson  
GL Hearn Ltd  
280 High Holborn  
London  
London  
W1CV 7EE

**RECOMMENDATION:** That upon completion of the s106 Legal Agreement the Head of Development Management/Planning Decisions Managers be Authorised to GRANT planning permission subject to conditions.

**Note for Members:**

Ref: 15/04513/FUL LOCATION: Monmouth Road Car Park, 35-36 North Square And North Ma



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## **1. Site and Surroundings**

- 1.1 The application site is a car park associated with Edmonton Green Shopping Centre. St. Martins Road is to the east of the site and Monmouth Road is to the north, to the west of the site is Edmonton Baptist Church and to the south and south west is the shopping centre.
- 1.2 Whilst the immediate site is a shopping centre, the surrounding area is generally residential in nature. On the opposite side of St. Martins Road and Monmouth Road there is two-storey terrace housing, there are four-storey residential blocks on the western side of Hertford Road and within the shopping centre site there are residential blocks, three of up to 25-storeys in height and up to eight-storeys above the Leisure Centre.
- 1.3 The site is not located in a Conservation Area and is not listed but The Crescent conservation area and Grade II Listed buildings are just to the north of the site. The site has a PTAL rating of 6a. The site is not located within a controlled parking zone and it is relatively flat lying.

## **2. Proposal**

- 2.1 The applicant seeks full planning permission for the erection of a 9-storey block to provide 77 residential units (comprising 28 x 1-bed and 49 x 2-bed flats) with balconies to the front and rear elevations and solar panels to the roof.
- 2.2 The proposal involves a reduction in parking spaces and the reconfiguration of the car park. Additionally the proposal includes the conversion of vacant

first floor commercial space (Class B1) to provide 5 x 3-bed residential units with amenity deck and refurbishment of North Mall retail thoroughfare.

- 2.3 The residential development would be located at the southwest corner of the car park, at its lower floors abutting the 99p store to the south. At its northern end the block is positioned 8.2m to the east of the adjacent church
- 2.4 Amended plans have been received during the course of the application and the council has carried out a re-consultation exercise to update neighbouring occupiers of these changes. The revisions included improvements predominantly to the design and to servicing arrangements.

### **3. Relevant Planning History**

- 3.1 15/02349/PREAPP  
Proposed construction of 63 residential units within a 9-storey block to car park site (18 x 1-bed, 45 x 2-bed) with undercroft car parking, conversion of first floor retail/office accommodation above 2-9 North Square to 5 x 3-bed residential units together with refurbishment of North Square.

### **4. Consultation**

#### Public Consultations

- 4.1 The original 21 day public consultation period started on the 14<sup>th</sup> October and concluded on 4<sup>th</sup> November. A site notice was posted close to the site and the application was also advertised in the local paper. At this stage there were four objections received from the public.
- 4.2 Following the receipt of a revised drawings it was necessary to re-consult local residents and this was undertaken in the week commencing 23<sup>rd</sup> May 2016. A petition objecting to the application with 40 signatures from the Monmouth Road area has been received, along with an objection from the adjacent church and 4 individual objections.

Concerns raised include:

- Impact on traffic and parking;
- The car park is extensively used each day by shoppers;
- Parking zones have recently been introduced around the north side of Edmonton green;
- Impact on light for neighbouring properties;
- Height of the proposal;
- Impact on the view of The Crescent (Listed Buildings);
- Strain on services in the area including North Middlesex Hospital;
- The development will add to disruption caused by cycle Enfield;
- Edmonton is neglected for services and open spaces and positive services for the community that already live here are needed;
- Overdevelopment of Edmonton;
- Overpopulation and overcrowding leading to increasing noise, crime, antisocial behaviour and alcoholism;
- Increase in rubbish on the roads;
- More congested roads will more dangerous for pedestrians;
- The area will feel more insecure and unsafe;

- There is hardly any greenery which may also disappear leaving tall concrete buildings;
- Concern about vehicular and pedestrian movement around the church;
- Uncomfortable relationship between the siting of the block and the church reduced to 10m;
- Pedestrians are unlikely to follow the designated route through the car park but will take the more direct route;
- Concern about servicing needs for new A1-A5 unit;
- The amended scheme shows no trees in the new square; and
- Concern from the church about the level of use and management of the route leading around the south and east of their building.

#### Internal

- 4.3 Urban Design - Concern regarding layout, quality of accommodation, lack of landscaping and open space, height, massing and detailing.
- 4.4 Environmental Health - No concerns regarding air quality or contaminated land but its needs to be demonstrated that an acceptable internal noise level is achieved.
- 4.5 Housing Officer - The over concentration of 1 and 2 bed units is not compliant and is unacceptable. 33 of the 82 units should be affordable housing.
- 4.6 Heritage - Concerns mounting to an objection to the proposal were received, discussed within the body of the report.
- 4.7 SUDS and Flood Risk - A sustainable drainage strategy is required and the September 2015 FRA is unacceptable as it proposed brownfield rather than greenfield runoff rates. This will be required by condition.

#### External

- 4.8 Thames Water - Consider the submitted information insufficient and propose conditions and informatives.
- 4.9 Environment Agency - No comments received.
- 4.10 Urban Design London - Concern about the lack of an overarching spatial plan to enable a comprehensive approach. PRS requires well managed and good servicing provision, did not support the proposal.

### **5. Relevant Policy**

#### 5.1 Development Management Document

DMD1	Affordable Housing on site capable of providing 10 or more units.
DMD3	Providing a Mix of Different Sized Homes
DMD6	Residential Character
DMD8	General Standards for New Residential Development
DMD9	Amenity Space
DMD10	Distancing

DMD22	Loss of Employment Outside of Designated Areas
DMD27	Angel Edmonton, Edmonton green, Southgate and Palmers green District Centres
DMD37	Achieving High Quality and Design-Led Development
DMD40	Ground Floor Frontages
DMD44	Conserving and Enhancing Heritage Assets
DMD45	Parking Standards and Layout
DMD47	New Roads, Access and Servicing
DMD48	Transport Assessments
DMD49	Sustainable Design and Construction Statements
DMD50	Environmental Assessment Methods
DMD51	Energy Efficiency Standards
DMD55	Use of Roof Space/Vertical Surfaces
DMD56	Heating and Cooling
DMD58	Water Efficiency
DMD59	Avoiding and Reducing Flood Risk
DMD60	Assessing Flood Risk
DMD61	Managing Flood Risk
DMD62	Flood Control and Mitigation measures
DMD64	Pollution Control and Assessment
DMD65	Air Quality
DMD66	Land Contamination and Instability
DMD72	Open Space Provision
DMD73	Children's Play Space
DMD79	Ecological Enhancements
DMD80	Trees on Development Sites
DMD81	Landscaping

## 5.2 Core Strategy

CP2	Housing supply and locations for new homes
CP3	Affordable housing
CP4	Housing quality
CP5	Housing types
CP16	Taking part in economic success and improving skills
CP17	Town Centres
CP20	Sustainable energy use and energy infrastructure
CP21	Delivering sustainable water supply, drainage and sewerage infrastructure
CP22	Delivering sustainable waste management
CP25	Pedestrians and cyclists
CP30	Maintaining and improving the quality of the built and open environment
CP31	Built and Landscape Heritage
CP32:	Pollution
CP39	Edmonton
CP46	Infrastructure Contribution

## 5.3 London Plan (March 2015) (FALP)

Policy 3.3	Increasing housing supply
Policy 3.4	Optimising housing potential
Policy 3.5	Quality and design of housing development
Policy 3.6	Children and young people's play and informal recreation facilities
Policy 3.8	Housing choice

Policy 3.9	Mixed and balanced communities
Policy 3.10	Definition of affordable housing
Policy 3.11	Affordable housing targets
Policy 3.12	Negotiating affordable housing on schemes
Policy 3.13	Affordable housing thresholds
Policy 5.2	Minimising carbon dioxide emissions
Policy 5.3	Sustainable design and construction
Policy 5.4	Retrofitting
Policy 5.7	Renewable energy
Policy 5.9	Overheating and Cooling
Policy 5.10	Urban greening
Policy 5.11	Green Roofs and Development Site Environs
Policy 5.13	Sustainable drainage
Policy 5.14	Water quality and wastewater infrastructure
Policy 5.15	Water use and supplies
Policy 5.16	Waste self sufficiency
Policy 6.9	Cycling
Policy 6.10	Walking
Policy 6.13	Parking
Policy 7.2	An inclusive environment
Policy 7.3	Designing out crime
Policy 7.4	Local character
Policy 7.5	Public realm
Policy 7.6	Architecture
Policy 7.8	Heritage Assets and Archaeology

#### 5.4 Other Relevant Policy

- National Planning Policy Framework

#### 5.5 Other Material Considerations

- The Mayors Housing SPG (2016)

### **6. Analysis**

6.1 The main issues for consideration regarding this application are as follows:

- Principle of the Development;
- Acceptability of PRS model;
- Scale and Density;
- Design and Impact on the Character of the Surrounding Area and Heritage Assets;
- Neighbouring Amenity;
- Proposed Type and Mix of Units;
- Standard of Accommodation and Private Amenity provisions;
- Traffic, Parking and Servicing Issues;
- Affordable Housing and other S106 Contributions;
- Sustainability; and

#### 6.2 Principle of the Development

6.2.1 The principle of a new residential development within Edmonton Green District Centre is considered to be acceptable subject to the development

being appropriate and complying with Council policies. Policy CP39 set out aspirations to enhance the areas liveability but also acknowledges that further housing, shops and employment will be required to reinforce and enhance the identity of the area. The proposal delivers housing within the area, and therefore partially contributes to the aim of this policy.

- 6.2.2 While at the pre-application stage, officers expressed a preference for a comprehensive plan for the regeneration and redevelopment of the whole site, together with the wider town centre, to ensure redevelopment and housing numbers are optimised, there is no in principle objection to the redevelopment of part of the car park. This does however require further detailed assessment, particularly in terms of urban design and parking/servicing.
- 6.2.3 The conversion of the redundant offices has been considered against policy DMD22. The planning statements explains that the offices were constructed in the 1970's and are now beyond their economic life, not having benefitted from any investment required to modernise the accommodation, they have therefore remained vacant for two years. A Market Demand and Viability Assessment which would usually be expected to demonstrate that marketing efforts have been undertaken to support a change of use, hasn't been received. However the proposed replacement residential use is appropriate in this location subject to compliance with Council policy.

### 6.3 Density

- 6.3.1 Policy 3.4 (Table 3.2) of the London Plan sets standards for appropriate density levels with regards to location, existing building form, massing, and having regard to the PTAL (Public Transport Accessibility Level) score. The site is considered to have an urban character and the PTAL in this instance is high at 6a, the density table therefore advocates up to 260 units per hectare.
- 6.3.2 The planning statement suggests that the density is 130 units per hectare, this is based on the inclusion of the entire site area which primarily consists of the retained car park which does not form part of the curtilage of the actual proposed residential development. As such this calculation is not considered to be a true reflection of the density of the proposal, a density of 130 units per hectare would in fact be an underdevelopment of this site in this location.



6.3.3 The development has no private curtilage outside of its footprint and based on the footprint as the site area the density is 716 units per hectare. If the site area is stretched out to include the areas identified on the block plan which are necessary for the servicing of the development, primarily to the north and west of the site, the density is around 295 units per hectare.

6.3.4 The method for the calculation of the density proposed by the Applicant can be argued, but in isolation officers consider the density of the proposal for this building to be in excess of the density thresholds for an urban area. However density is not the only consideration and the quality of a development and the contribution it makes to public benefit can justify the support of higher densities. This consideration is set out below.

#### 6.4 Design and Impact on the Character of the Surrounding Area and Heritage Assets

6.4.1 The proposed new development is a nine-storey block and as set out above the context of the site varies greatly from two-storey terrace houses to tower blocks of over 20 storeys in height. These do establish a precedent for height above the nearby traditional houses, especially as that proposed is contiguous with the Edmonton Green shopping centre.

6.4.2 The siting of the development within the car park was the subject of some consideration as set out in the design evolution section of the D&A. Perimeter blocks were explored to relate to the character of surrounding streets and this was supported by officers, but the tighter relationship with neighbouring dwellings were considered to impose limitations on the quantum of development the applicant sought to viably achieve. It would also impact more significantly on the quantum of parking which could be retained. The location selected in the southwest corner of the car park, and adjacent to the town centre uses, maximises the retained car parking spaces.

6.4.3 In response to officer's comments the design of the block has changed quite considerably during the lifetime of the current application. The proposal consists of a brick building nine storeys in height with the flat roofed top floor recessed from the northern and southern elevations. Floor to ceiling style windows follow consistent rhythms around the elevations and glazed balconies and coloured panels adjacent to some windows add some further elevational detail. The use of brick is welcomed and to ensure a successful finished appearance officers recommend a condition regarding the depth of window reveals.

6.4.4 Although neither side of the block fronts a road, the proposed orientation envisages its front elevation on the western side, which is the side closest to the bus and train stations and so most logical for pedestrian access. The rear elevation abutting the public car park provides secondary access as well as access to the undercroft disabled parking spaces, cycle stores and the plant room.

6.4.5 Officers have worked with the architect to seek to improve the proposed public realm around the development. A small retail/café unit is proposed at ground floor in the front elevation which may introduce some activity and connect the site to the adjacent town centre uses. While trees are being

removed it is important that they are replaced to meet local and London policy and they can be accommodated both within the small landscaped area at the main entrance to the block and potentially around the existing car park.

6.4.6 The building is set well in from the surrounding roads (42m from the north, 30m from the east and 69m from the west) but the car park and adjacent low rise retail and church buildings do not screen the nine-storey block. The nearby development consisting of the leisure centre and flats above, opposite Edmonton Green station, fronts the street directly and has a very prominent appearance. The proposal whilst taller will not directly dominate the street in the same way and will be of a slightly better quality of architecture, particularly with the use of brick. The views of the proposal submitted with the initial design, can be used to understand the visibility of the development in the streetscene. Though large and prominent it is not considered overbearing primarily due to its location within a car park and lack of immediate residential neighbours or street-fronting location, and the revised design has reduced the bulk form the original submission.

6.4.7 The NPPF sets out that local planning authorities should identify and assess the significance of any heritage asset that may be affected and take this into account when considering the impact of the proposal. Therefore, the proposal also has to be assessed with regard to any impact it may have on the conservation area and listed buildings, The Crescent, to the north. The consistent parapet line of The Crescent is one of its important features and concern was raised that the proximity of the nine-storey building projecting above this would harm this feature. The council's heritage officers expressed concern that insufficient analysis of the application and its impact on the nearby heritage assets has been undertaken particularly including how views of the development could affect the heritage setting in terms of both 'key' and longer views from Hertford Road.

6.4.8 Para. 134 of the NPPF states that where a development will lead to some harm to the significance of a designated heritage asset, but the harm is considered to be less than substantial, this should be weighed against the public benefits of the proposal.

6.4.9 The original D&A sets out that the conservation area is 65m from the development and the closest building is 100m away. The siting of the building back from the main road means that the main corridor view is not directly affected but the development will be visible in the skyline above the roofs at the southern end of The Crescent. The tower blocks are already prominent in this view, the proposed development is significantly closer and may appear bulkier but it is much lower and trees help to some extent to disguise it. The harm is considered to be less than substantial while the development delivers both new housing and the refurbishment and uplift of the appearance of the North Mall.

## 6.5 Impact on Neighbouring Amenity

6.5.1 The significant separation distances between the proposed building and the surrounding roads, set out above, prevent any impact on overlooking or privacy. However the building still has potential to impact on the daylight and sunlight experienced by surrounding residents.

6.5.2 In the daylight/sunlight report submitted with the application the assessor concludes that the development would not adversely impact on daylight and sunlight amenity received by neighbouring residents. Following the revision proposal a supplementary letter was provided expressing the conclusion that the amendments including the introduction of a set in to the top floor at the northern and southern end of the building, the repositioning of the development 1.5m to the west and 2m to the north would not materially impact on the initial findings and may in some instances improve the relationship.

## 6.6 Type and Mix of accommodation

### *Type of accommodation*

6.6.1 The application proposes a residential block purpose built for private rent, known as build to rent or PRS, the vacant offices are also proposed to be converted for the same use. The London Plan (2015) policy 3.8 views this tenure of development positively and supports growth in private renting where this will result in well managed, good quality accommodation, mixed and balanced communities and sustainable neighbourhoods.

6.6.2 The proposed privately rented units will be let and managed by the applicant. One suggested benefit of this type of rented accommodation is an assurance of the quality of the management which can be a problematic issue elsewhere in the sector for example with rogue or inexperienced landlords. The s106 will detail the requirements of the applicant with regard to management, lettings and rent increases.

### *Housing Mix*

6.6.3 DMD 3 and Policy 5 of the Core Strategy seeks new development to incorporate a mix of dwelling types and sizes to meet housing needs in the Borough with family sized accommodation (3 bed or larger) is the greatest area of need. The Council's dwelling mix ratios are as follows:

1 and 2 person flats - 20%  
2 bed flats - 15%  
3 bed houses - 45%  
4 + bed houses - 20%

6.6.4 The development across the two sites will result in 82 units providing the following dwelling mix:

28 x 1 bed (34%)  
49 x 2 bed (60%)  
5 x 3 bed (6%)

6.6.5 Rather than providing 65% family size units the development provides 6%, the family units are all accommodated within the converted office units rather than in the new building. It is however accepted that the type of development proposed, for private renters with minimal access to amenity space, would be poorly suited for family accommodation.

- 6.6.6 It is also relevant that although the proposal provides a low level of family sized accommodation, the surrounding area has a high number of family sized houses, and in the round this is considered to balance out.
- 6.6.7 Given the current London Plan's support or developments for private rent, prior to any review undertaken by the new Mayor, the application, subject to terms in the legal agreement securing details of marketing and management, the proposal is on balance acceptable.

## 6.7 Standard of Accommodation and Private Amenity provisions

### *Quality of accommodation*

- 6.7.1 Policy 3.5 of the London Plan specifies that 1 bed flats should a minimum floor area of 50sqm, 2 bed flats should have a minimum internal floor area of 61 square metres with 3b4p flats at 74sqm or 3b6p flats at 86sqm. All units have been measured and verified and are above the required London Plan standards for the respective units. From assessing the plans all units would have useable and accessible layouts and all room sizes are acceptable with specific regards to living/diners and single and double bedrooms. While the vast majority of the units (60%) are single aspect which would normally be considered unacceptable due to the poor outlook, ventilation and daylight/sunlight this provides residents, as none of these are single aspect north facing, and this new development would potentially help regeneration, this is considered sufficient in this instance.
- 6.7.2 The D&A confirms that 10% of the units will be wheelchair accessible.

### *Private Amenity*

- 6.7.3 London Housing SPG and the Enfield's adopted Development Management Document, set out appropriate amenity space standards for residential developments.
- 6.7.4 Policy DMD9 specifies the requirements for private and communal amenity space for such developments and the London Housing SPG seeks 5sqm of private space for each unit.
- 6.7.5 Each of the proposed flats within the new building would be served by its own balcony, or terraces on the upper floor, achieving the minimum 5sqm requirement. There is no communal amenity space proposed due to the tight confines of the site.
- 6.7.6 The larger family sized three bed units in the converted building have no private amenity space, they are accessed via an external deck which could to some extent be utilised as amenity space though given the rented nature of the development the extent to which residents will invest in enhancing the usability of this space may be limited. These units do however benefit from a generously sized communal amenity space on a deck. No detail is provided of how this space will be screened or treated, so full details, including planters capable of accommodating mature shrubs, shall be required by condition.

## 6.8 Traffic and Transportation

### *Implication for existing car park*

- 6.8.1 The proposal will result in the loss of 39 spaces in the existing car park. Transportation officers recommend the requirements of a contribution of £30,000 towards the review, consultation and implementation of changes to the management of on-street parking within reasonable walking distance from the site to mitigate the impact of potential overspill parking.

#### *Pedestrian Access*

- 6.8.2 The revised arrangement has improved the proposed pedestrian access from an arrangement which was considered to be unacceptable. A designated route through the car park is indicated though full details are required by condition. The location of the front entrance and introduction of a small retail type unit may provide some activity and sense that the entrance is visible and safe.

#### *Access for servicing*

- 6.8.3 The proposals for servicing and drop-offs originally envisaged that vehicles would enter and exit from the same point. This would require vehicles to manoeuvre in front of the building in the area which would also be used by pedestrians and cyclists. This arrangement was considered to be unacceptable. The revised proposal utilises the entrance from the east and proposes a one-way through route exiting to the north. The one way traffic and the removal of the need to manoeuvre is much more conducive to a safe environment for pedestrians accessing the building.

#### *Car Parking*

- 6.8.4 In a new residential development it would usually be anticipated that parking should be provided for each unit. In this location the PTAL 6a is very good so parking provision would be expected to be at the lowest level. It is, however, proposed that the development will be car free in that no parking is proposed for the residents, with the exception of the provision of 10 wheelchair accessible parking spaces accommodated in the undercroft of the block. The level of access to public transport makes this acceptable in principle.
- 6.8.5 It has been suggested that there could be scope for residents to rent spaces in the multi-storey car park but this does not form part of the planning application.
- 6.8.6 Whilst it is not currently in a CPZ the site is adjacent to the boundary of the extended Edmonton Green CPZ. It is considered that in an area of high parking stress, the development should be car free with the exception of the disabled parking and should be exempt from any future CPZ, this would be secured within the s106.

#### *Cycle Parking*

- 6.8.7 The provision of two-tier cycle storage is not supported by Transportation officers as it is not easy or convenient to use is not considered appropriate for residential developments. However it is unlikely that the quality of cycle storage could otherwise be achieved, conditions will be required by condition allowing the specifics of the design to be considered. 82 cycle storage spaces are proposed as two-tier and another store of 22 Sheffield stand allowing the

accommodation of a further 44 bikes. Resulting in a total capacity for 126 bikes which is sufficient to meet the London Plan standards for the mix of one and two bed units in the new block.

6.8.8 £26,667 is sought towards the provision of pedestrian/cycle infrastructure in the immediate vicinity of the site as part of the cycle Enfield project.

*Waste*

6.8.9 The proposed bin store, in the ground floor frontage, shows capacity for 20 large bins which is in line with the Council's requirements as set out in Enfield's Waste and Recycling Planning Storage Guidance (ENV-08-162):

Number of Properties	Number of Containers required for Refuse:	Number of Containers required for Recycling:
20+ units	4 x 1100 litre bin per 20 properties	1 x 1280 litre bin per 20 properties

These containers must be:

- Within 10 metres of the collection point;
- Bins must be stored on a hard surface or in a storage cupboard;
- Bins that are stored in a storage cupboard must be housed in chambers constructed in accordance with the British Standard Code of Practice BS 5906:1980 "Storage and On-Site treatment of solid waste from buildings"; and
- Footpaths between the container housing and the nearest vehicular access should be free from steps or kerbs, have a solid foundation, have a smooth solid surface, be level and have a gradient no more than 1:12 and a minimum width of 2 metres.

6.9 S106 Contributions and Obligations

*Affordable Housing*

6.9.1 Having regard to policies DMD1 and CP3 of the Core Strategy as the site is proposing 10 or more units (82) it should be complying with borough wide target of achieving 40% affordable housing and a mix of tenures to reflect a borough wide target of 70% social rent and affordable rent and 30% Intermediate.

6.9.2 However the NPPG sets out the developments built for private rent differ from build to sale developments in their economics and should be determined on a case by case basis. Including determining when affordable housing payments may be required to ensure the developments remain viable.

6.9.3 As part of the original submission the applicant has submitted a Viability Assessment that concluded that the scheme would not be viable to contribute on-site affordable units. This Viability Assessment was assessed by the councils own independently appointed Viability Assessor and was accepted.

6.9.4 The council is responsible for ensuring that local residents benefit from private development and that the maximum viable amount of affordable housing is achieved. In the case of development building for private rent elsewhere an approach has been developed to ensure that, at the point in time in the future (after the agreed 15 years) when the units may be switched

to sale a viability assessment will be undertaken and appropriate contributions towards affordable housing will be secured.

#### *Education Contributions*

- 6.9.5 Having regard to policy CP46 of the Core Strategy and the councils S106 SPD, this application would also be required to provide education contributions.

#### *Highways*

- £22,667 towards cycle route improvements and pedestrian environment improvements;
  - £30,000 towards the review, consultation and implementation of changes to the management of on-street parking within reasonable walking distance from the site;
  - One three year car club membership per unit and driving credit of £100 per membership; and
  - Exception from any future CPZ and car free with the exception of disabled parking.
- 6.9.6 S106 Monitoring fees to be agreed.

#### *PRS*

- 6.9.7 The following obligations are recommended for the specific type of accommodation proposed:
- Provide the units as private rented sector units for a minimum of 15 years (at which point, if the use is proposed to change, a viability review will be undertaken);
  - Marketing strategy covering the prioritisation of those who live and or work in the borough and demonstrating local marketing including in council publications; and
  - Private Rented Sector Management plan covering: length of initial lease a minimum of 12 months which can subsequently be renewed for up to 5 years at the owners discretion; consistent and quality level of housing management; rent increases, which shall reflect the market and not exceed one in any 12 month period.

#### *Employment and Skills Strategy*

- An employment and skills strategy (tba)

### 6.10 Sustainability

#### *Sustainable Design and Construction*

- 6.10.1 Appendix 3 of the DMD sets out the broad range of issues considered under this theme and this has been considered to some extent in the application submission. The provision for waste and recycling storage for future residents is set out above and the energy statement confirms that targets are achieved. The drainage details submitted with the application are unacceptable and a condition setting out the requirements is recommended.

The proposal hasn't explored ways in which the development could support improvements to ecology or urban greening, but the latter is included within a recommended condition.

#### *Lifetime Homes*

- 6.10.2 The London Plan and Core Strategy confirm that all new housing is to be built to Lifetime Homes' standards. Confirmation of this should be secured by condition/s106.

#### *Energy / Energy efficiency*

- 6.10.3 The London Plan adopts a presumption that all developments will meet carbon dioxide emission reductions that will improve upon 2013 Building Regulations, leading to zero carbon residential buildings from 2016. Policy 5.2 establishes a target to be a 35% improvement over Part L of current Building Regulations

#### *Water efficiency*

- 6.10.4 Requirements are recommended by condition.

#### 6.11 CIL

- 6.11.1 The development would be liable to a Community Infrastructure Levy contribution as the size exceeds 100 sq.m. The net gain of the new created floor area is 7,361.22sq.m, inclusive of the 5 units proposed through the office space conversion.

- 6.11.2 As a result the borough CIL payment would be 7,361.22sq.m x £40 per sqm (CIL Rate for Edmonton Area) = £295,786.00

- 6.11.3 This would result in a Mayoral CIL contribution of 7,361.22sq.m x £20 = £147,224.40.

## **7 Conclusion**

- 7.1 Although officers had looked to explore comprehensive redevelopment options, it is recognised that this scheme has to be considered on its merits taking into account the relationship to and continued regeneration of Edmonton Green shopping centre.

- 7.2 The development would deliver homes based on a private rent model (PRS), and maintained for this use for at least 15 years. No affordable housing is proposed but the development may in the future provide a contribution towards affordable housing delivery.

- 7.3 The design and appearance of the new block is acceptable and the impact on the nearby listed buildings is considered to be less than substantial.

- 7.4 As such, officers consider the scheme to be on balance, acceptable, subject to the conditions outlined as below and the completion of an acceptable s106 Legal Agreement.



## **Recommendation**

- 8.1 On balance, it is recommended that planning permission be granted subject to conditions and signing of the s106.

## **Conditions:**

### ***3 Years***

1. The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of the decision notice.

Reason: To comply with the provisions of S.51 of the Planning & Compulsory Purchase Act 2004.

### ***Approved Plans***

2. The development hereby permitted shall be carried out in accordance with the approved plans, as set out in the attached schedule which forms part of this notice.

Reason: For the avoidance of doubt and in the interests of proper planning.

### ***Construction Management Plan***

3. That development shall not commence until a construction methodology has been submitted to and approved in writing by the Local Planning Authority. The construction methodology shall contain:
- a. arrangements for wheel cleaning;
  - b. arrangements for the storage of materials;
  - c. hours of work;
  - d. arrangements for the securing of the site during construction;
  - e. the arrangement for the parking of contractors' vehicles clear of the highway;
  - f. The siting and design of any ancillary structures;
  - g. Arrangements for the loading and unloading of plant and materials;
  - h. Scheme for recycling/disposing of waste resulting from demolition and construction works;
  - i. Enclosure hoarding details; and
  - j. Measures that will be taken to control dust, noise and other environmental impacts of the development in accordance with 'London Best Practice Guidance: The control of dust and emission from construction and demolition'.

The development shall be carried out in accordance with the approved construction methodology unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not lead to damage to the existing highway and to minimise disruption to neighbouring properties and the environment.

### ***Contamination***

4. Prior to the commencement of building works, a scheme to deal with the risks associated with contamination of the site shall be submitted to and approved, in writing, by the local planning authority. That scheme shall include all of the following elements unless specifically excluded, in writing, by the Local Planning Authority and the development shall then proceed in strict accordance with the measures approved.
  - a. A desk study identifying: all previous uses; potential contaminants associated with those uses; a conceptual model of the site indicating sources, pathways and receptors; potentially unacceptable risks arising from contamination at the site;
  - b. Site investigation scheme, based on (1) to provide information for an assessment of the risk to all receptors that may be affected, including those off site;
  - c. The results of the site investigation and risk assessment (2) and a method statement based on those results giving full details of the remediation measures required and how they are to be undertaken.
  - d. A verification report on completion of the works set out in (3) confirming the remediation measures that have been undertaken in accordance with the method statement and setting out measures for maintenance, further monitoring and reporting.

Any changes to these agreed elements require the express consent of the Local Planning Authority.

Reason: In order to ensure that the development does not pose an unacceptable risk to the quality of the groundwater.

### ***Sustainable Drainage Strategy***

5. The development shall not commence until a Sustainable Drainage Strategy has been submitted to and approved in writing by the Local Planning Authority. The details shall be based on the disposal of surface water by means of a sustainable drainage system in accordance with the principles as set out in the Technical Guidance to the National Planning Policy Framework and should be in line with our DMD Policy SuDS Requirements:
  - a. Shall be designed to a 1 in 1 and 1 in 100 year storm event with the allowance for climate change;
  - b. Follow the SuDS management train and London Plan Drainage Hierarchy by providing a number of treatment phases corresponding to their pollution potential;
  - c. Should maximise opportunities for sustainable development, improve water quality, biodiversity, local amenity and recreation value;
  - d. The system must be designed to allow for flows that exceed the design capacity to be stored on site or conveyed off-site with minimum impact;

- e. Clear ownership, management and maintenance arrangements must be established; and
- f. The details submitted shall include levels, sizing, cross sections and specifications for all drainage features.

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy and Policies 5.12 & 5.13 of the London Plan and the NPPF and to maximise opportunities for sustainable development, improve water quality, biodiversity, local amenity and recreation value.

- 6. Prior to occupation of the development, a Verification Report demonstrating that the approved drainage / SuDS measures have been fully implemented shall be submitted to the Local Planning Authority for approval in writing. This report must include:
  - a. As built drawings of the sustainable drainage systems;
  - b. Level surveys of completed works;
  - c. Photographs of the completed sustainable drainage systems;
  - d. Any relevant certificates from manufacturers/ suppliers of any drainage features;
  - e. A confirmation statement of the above signed by a chartered engineer.

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy and Policies 5.12 & 5.13 of the London Plan and the NPPF.

#### ***Piling Method Statement***

- 7. No impact piling shall take place until a piling method statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to and approved in writing by the local planning authority in consultation with Thames Water. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

Reason: The proposed works will be in close proximity to underground sewerage utility infrastructure. Piling has the potential to impact on local underground sewerage utility infrastructure. The applicant is advised to contact Thames Water Developer Services on 0845 850 2777 to discuss the details of the piling method statement.

#### ***Detailed Drawings***

- 8. Detailed drawings to a scale of 1:20 to confirm the detailed design and materials of the:
  - a. Schedule and sample of materials used in all elevations, should also include brick/cladding sample board (bonding and pointing);

- b. Details of all windows and doors at scale 1:10, windows shall be set at least 115mm within window reveal scale 1:10;
- c. Details of balconies, bris soliel and canopies;
- d. Construction details of all external elements at 1:20 scale (including sections). This should include: entrances and exits, glazing, masonry, weathering and flashings, balustrades and parapets, roof, plant and plant screening, health and safety systems;
- e. Full drawn details (1:20 scale elevations, 1:2 scale detailing) of the railings, gates and shutters (including hinges, fixings, locks, finials);
- f. Details and locations of rain water pipes; and
- g. Details of treatment of and screening of amenity deck including planters and seating.

Shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of development above ground hereby permitted. The development shall thereafter be carried out solely in accordance with the approved details.

Reason: To safeguard and enhance the visual amenities of the locality.

### ***Samples and Materials***

- 9. Prior to commencement of development above ground hereby approved, a sample panel and a schedule of materials to be used in all external elevations including walls, doors, windows front entrances and balconies within the development hereby permitted shall be submitted to and approved in writing by the Local Planning Authority before any building work commences and this condition shall apply notwithstanding any indications as to these matters which have been given in the application. The development shall thereafter be carried out solely in accordance with the approved details.

Reason: In order to ensure that the building has an acceptable external appearance and preserves the character and appearance of the conservation area.

### ***Surfacing Materials***

- 10. Prior to the commencement of development other than the super structure, details and design of the surfacing materials to be used within the development including footpaths, shared surfaces, access roads, parking areas, road markings and all other hard surfacing shall be submitted to and approved in writing by the Local Planning Authority. The surfacing shall be carried out in accordance with the approved detail before the development is occupied or use commences.

Reason: To ensure that the development does not prejudice highway safety and a satisfactory appearance.

- 11. Prior to the commencement of buildings works above grade, a Landscape and Public Realm Strategy for all external public realm areas within the curtilage of the site hereby approved shall be submitted to and approved by the Local Planning Authority. This Strategy is to include, amongst other things, details of proposed plant and tree maintenance, paving materials, pedestrian priority materials and shared surface treatments, plant species, ground levels, green roofs, boundary treatments and water features. The

development shall be in accordance with the approved details, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that the proposed landscaping areas are of a high quality and for consistent treatment of the public realm.

### ***Soft Landscaping***

12. Prior to the commencement of development other than the super structure, details of trees, shrubs, grass and all other soft landscaped areas of internal and external amenity spaces (including roof terraces) to be planted on the site shall be submitted to and approved in writing by the Local Planning Authority. The planting scheme shall be carried out in accordance with the approved details in the first planting season after completion or occupation of the development whichever is the sooner. Any planting which dies, becomes severely damaged or diseased within five years of planting shall be replaced with new planting in accordance with the approved details.

Reason: To provide a satisfactory appearance and ensure that the development does not prejudice highway safety.

### ***Levels***

13. The development shall not commence until plans detailing the existing and proposed ground levels including the levels of any proposed buildings, roads and/or hard surfaced areas have been submitted to and approved in writing by the Local Planning Authority. The development shall be constructed in accordance with the approved details.

Reason: To ensure that levels have regard to the level of surrounding development, gradients and surface water drainage.

### ***Lighting Strategy***

14. Prior to occupation, full details of a lighting strategy, including details of the lighting of all public areas, cycle stores and buildings shall be submitted to and approved in writing by the Local Planning Authority. The approved lighting shall be installed before the commencement of the use and maintained thereafter. Development shall be implemented and retained in accordance with the approved details.

Reason: To ensure that the local Planning Authority may be satisfied with the details of the proposal.

### ***Refuse Stores***

15. The development shall not be occupied until details of refuse storage facilities including facilities for the recycling of waste to be provided within the development, in accordance with the London Borough of Enfield Waste and Recycling Planning Storage Guidance ENV 08/162, have been submitted to and approved in writing by the Local Planning Authority. The facilities shall be provided in accordance with the approved details before the development is occupied or use commences.

Reason: In the interests of amenity and the recycling of waste materials in support of the Boroughs waste reduction targets.

### ***Cycle Parking***

16. The development shall not be occupied until details of the siting, number and design of secure/covered cycle parking spaces have been submitted to and approved in writing by the Local Planning Authority. The approved details shall thereafter be installed and permanently retained for cycle parking.

Reason: To ensure the provision of cycle parking spaces in line with the Council's adopted standards.

### ***Energy Statement***

17. The development shall not commence until a detailed 'Energy Statement' and relevant SAP calculations has been submitted and approved in writing by the Local Planning Authority. Submitted details will demonstrate the energy efficiency of the development and shall provide for no less than 11% total CO2 emissions arising from the operation of a development and its services over Part L of Building Regs 2010 ensuring that standard conversion factor indicate that natural gas is the primary heating fuel. The Energy Statement should outline how the reductions are achieved through the use of Fabric Energy Efficiency performance, energy efficient fittings, and the use of renewable technologies.

The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter. Following practical completion of works a final Energy Performance Certificate shall be submitted to an approved in writing by the Local Planning Authority. Where applicable, a Display Energy Certificate shall be submitted within 18 months following first occupation.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets are met in accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF.

### ***Energy Performance Certificate***

18. Following practical completion of works a final Energy Performance Certificate shall be submitted to an approved in writing by the Local Planning Authority prior to occupation of the development.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets are met in accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF.

### ***CHP Details***

19. Prior to the commencement of buildings works above grade, full details of the locations of the proposed CHP facilities, including all associated storage, and a strategy for managing deliveries to these facilities and details of how the CHP can connect up to a future Energy Centre/Decentralised Energy Network

for the area, are to be submitted to and approved in writing by the Local Planning Authority and the development shall be constructed and operated thereafter fully in accordance with the approved details.

Reason: In the interests of securing a sustainable development.

### ***Residential Sound Insulation***

20. Prior to the commencement of buildings works above grade, full details of sound insulation for the residential units which shall show how the building has been designed to meet the following standards, and the number and location of the units which shall have noise measurements carried out shall be submitted to and approved in writing by the Local Planning Authority:
  - a. For living rooms, 35 dB(A) LAeq 16 hour between 07:00 and 23:00 hrs;
  - b. For bedrooms, 30 dB(A) LAeq 8 hour between 23:00 and 07:00 hrs; and
  - c. 45 dB(A) max for any individual noise event (measured with F time weighting) between 23.00 and 07.00hrs.

The development shall thereafter be carried out in accordance with the approved details.

Reason: To ensure that no nuisance or disturbance is caused to the detriment of the amenities of future occupiers.

### ***No Amplified Sound***

21. There shall be no amplified sound, speech or music system fixed, used, or audible outside the ground floor commercial premises (A1/A2/A3).

Reason: To ensure that no nuisance or disturbance is caused to the detriment of the amenities of neighbouring occupiers.

### ***Use Sound Insulation***

22. Prior to the commencement of buildings works above grade, full details of the soundproofing of premises and insulation of premises (including ventilation and flue systems) for all party walls and the ceiling/floor between the commercial floorspace, car parking and other common parts) and the residential use floors, shall be submitted to and approved in writing by the Local Planning Authority, and thereafter be retained for the duration of the use, to prevent fumes, smell and noise permeating into adjoining accommodation.

Reason: To ensure that no nuisance or disturbance is caused to the detriment of the amenities of neighbouring occupiers.

### ***Plant Equipment***

23. Prior to the commencement of buildings works above grade, full details of internal and external plant equipment and trunking, including building services plant, ventilation and filtration equipment, commercial kitchen exhaust ducting/ventilation (*if applicable*), shall be submitted to and approved in

writing by the Local Planning Authority prior to the commencement of development. All flues, ducting and other equipment shall be installed in accordance with the approved details prior to the use commencing on site and shall be retained for the duration of the use.

Reason: To ensure that no nuisance or disturbance is caused to the detriment of the amenities of neighbouring occupiers.

***No Pipes***

24. No pipes or vents (including gas mains and boiler flues) shall be constructed on the external elevations unless they have first been submitted to the Local Planning Authority and approved in writing. Any pipes and vents shall be installed as approved.

Reason: Such works would detract from the appearance of the building and would be detrimental to the visual amenities of the locality.

***Servicing Management Plan***

25. Prior to occupation of the development, full details of a servicing management strategy for the management of deliveries and servicing of the development, shall be submitted to and approved by the Local Planning Authority. Servicing shall thereafter be carried out solely in accordance with the approved details.

Reason: In order that the Local Planning Authority and Transport for London may be satisfied as to the effects of the scheme on the adjacent road network so as to avoid hazard or obstruction to the public highways.

***Delivery Times***

26. No deliveries shall be taken to, or dispatched from, the commercial elements hereby permitted other than between the hours of 08.00 and 22.00 Mondays to Saturdays, and at no time on Sundays, Bank Holidays or Public Holidays, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To protect the amenities of future residential occupiers and the surrounding area.

***Travel Plan***

27. A Sustainable Travel Plan shall be submitted to and approved in writing by the Local Planning Authority prior to the use hereby permitted commencing. The measures approved in the Travel Plan shall be implemented prior to the residential use hereby permitted commencing and shall be so maintained for the duration of the use, unless the prior written approval of the Local Planning Authority is obtained to any variation.

Reason: To ensure that the travel arrangements to the residential development are appropriate and to limit the effects of the increase in travel movements.

***Commercial Floorspace b4 Residential***



28. Prior to the occupation of the residential units hereby permitted, the ground floor commercial floorspace shall be completed and available for occupation in accordance with the plans hereby approved, unless otherwise approved in writing by the Local Planning Authority.

Reason: To ensure the satisfactory provision of employment floorspace on the site.

***Hours of Commercial Operation***

29. The hours of operation of the ground floor commercial floorspace (A Use Classes) hereby permitted shall be submitted to and agreed in writing by the Local Planning Authority prior to first occupation of the relevant premises and shall not operate other than in accordance with the agreed hours unless agreed in writing by the Local Planning Authority.

Reason: To protect the amenities of adjoining occupiers.

***Wheelchair Adaptable Housing***

30. At least 10% of the residential flats hereby permitted shall be designed so that they can be easily adaptable to meet the Wheelchair Housing standard.

Reason: To secure appropriate access for disabled people.

***Lifetime Home Standards***

31. All the units shall comply with Lifetime Home standards in accordance with details to be submitted to and approved in writing by the LPA. The development shall be carried out strictly in accordance with the details approved and shall be maintained thereafter.

Reason : To ensure that the development allows for future adaptability of the home to meet with the needs of future residents over their life time in accordance with Policy CP4 of the Core Strategy and Policy 3.5 of the London Plan 2011.

***Crime Prevention Strategy***

32. Notwithstanding the details of the development, hereby approved, a detailed crime prevention management and maintenance strategy detailing how the development will minimise opportunities for crime including details of a controlled access system, CCTV and external lighting shall be submitted to and approved in writing by the Local Planning Authority prior to the first occupation of the development.

Reason: To ensure that the development protects community safety.

***Waste Water***

33. Development shall not commence until a drainage strategy detailing any on and/or off site drainage works, has been submitted to and approved by, the local planning authority in consultation with the sewerage undertaker. No discharge of foul or surface water from the site shall be accepted into the

public system until the drainage works referred to in the strategy have been completed.

Reason: The development may lead to sewage flooding; to ensure that sufficient capacity is made available to cope with the new development; and in order to avoid adverse environmental impact upon the community.

## **Informatives**

- 1 This decision letter does not convey an approval or consent which may be required under any enactment, by-law, order or regulation, other than Section 57 of the Town and Country Planning Act 1990.
- 2 You are advised that this consent is without prejudice to any rights which may be enjoyed by any tenants/occupiers of the premises.
- 3 Your attention is drawn to the provisions of the Building Regulations, and related legislation which must be complied with to the satisfaction of the Council's Building Control Officer.
- 4 You are advised to consult the Council's Environmental Health Division concerning compliance with any requirements under the Housing, Food, Safety and Public Health and Environmental Protection Acts and any by-laws or regulations made there under.
- 5 You are advised to consult the Council's Environmental Health Division with regard to the extraction of fumes from the premises.
- 6 Your attention is drawn to the necessity to register your food business with the Council's Environmental Health Division, under the Food Premises (Registration) Regulations 1991 before the use commences. Failure to do so may result in prosecution.
- 7 Your attention is drawn to Sections 4 and 7 of the Chronically Sick and Disabled Persons Act 1970 and the Code of Practice for Access for the Disabled to Buildings (B.S. 5810:1979) regarding the provision of means of access, parking facilities and sanitary conveniences for the needs of persons visiting, using or employed at the building or premises who are disabled.
- 8 You are advised of the necessity to consult the Transport and Highways team within the Transport Division of the Directorate of Environmental Services, with regard to any alterations affecting the public footway.
- 9 You are advised of the necessity to consult the Council's Refuse team within the Environmental Health with regard to the provision of refuse storage and collection facilities.
- 10 You are advised that this permission does not authorise the display of illuminated advertisements at the premises and separate consent may be required from the Local Planning Authority under the Town and Country Planning (Control of Advertisements) Regulations 1992.

- 11 You are advised that this permission does not authorise the display of advertisements at the premises and separate consent may be required from the Local Planning Authority under the Town and Country Planning (Control of Advertisements) Regulations 1992.
- 12 You are advised that permission will be required for the installation of a new shopfront at these premises.
- 13 In connection with the soundproofing condition, you should consult the Council's Building Control Section before carrying out any works.
- 14 You are advised of the necessity to consult Transport for London via email ([centrallicensing@tfl.gov.uk](mailto:centrallicensing@tfl.gov.uk)), prior to the commencement of construction in order to obtain necessary approvals and licences prior to undertaking any works within the public highway including scaffolding, temporary/permanent crossovers, oversailing/undersailing of the highway, drainage/sewer connections, hoarding, excavations (including adjacent to the highway such as basements, etc), temporary full/part road closures, craneage licences etc.
- 15 Any excavation or new groundworks at street or basement level associated with this development may expose or damage archaeological remains. Such works include the removal of the existing slab and/or foundations, construction of new foundations or floor slabs, lift pits, underpinning or service trenches.
- 16 The Local Planning Authority wishes to ensure that archaeological remains on this site are preserved in situ. The detailed proposals should include appropriate drawings, technical notes and method statements, showing how the objectives of in situ preservation are to be achieved. Particular attention should be paid to the design of foundations and new groundworks including piling, underpinning, new slab levels and slab construction, lift pits and new service trenches. You are advised to contact English Heritage's Archaeological Officers to discuss the submission of details required to discharge the archaeological condition(s).
- 17 Piling or other penetrative methods of foundation design can cause unacceptable risks to groundwater. A risk assessment should be carried out as per our guidance 'Piling into Contaminated Sites'. This and other guidance on piling in relation to condition 42 and into potentially contaminated sites is available at [www.environment-agency.gov.uk](http://www.environment-agency.gov.uk). Click on the publications and search for piling. The applicant is also advised to contact Thames Water Developer Services on 0845 850 2777 to discuss the proposals.
- 18 The water table is likely to be shallow at the site. It is therefore susceptible to pollution from oils and fuels from construction machinery.
- 19 The Environment Agency strongly recommends that the applicant consults our Pollution Prevention Guidance notes (PPGs). These are aimed at a wide range of industries and activities that have the potential to cause pollution. They can be downloaded from our website [www.environment-agency.gov.uk](http://www.environment-agency.gov.uk)
- 20 Surface Water Drainage - With regard to surface water drainage it is the responsibility of a developer to make proper provision for drainage to ground, water courses or a suitable sewer. In respect of surface water it is

recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of Ground Water. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be contacted on 0845 850 2777.

- 21 Where a developer proposes to discharge groundwater into a public sewer, a groundwater discharge permit will be required. Groundwater discharges typically result from construction site dewatering, deep excavations, basement infiltration, borehole installation, testing and site remediation. Groundwater permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 8507 4890 or by emailing [wwriskmanagement@thameswater.co.uk](mailto:wwriskmanagement@thameswater.co.uk). Application forms should be completed on line via [www.thameswater.co.uk/wastewaterquality](http://www.thameswater.co.uk/wastewaterquality). Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991.
- 22 Thames Water would recommend that petrol / oil interceptors be fitted in all car parking/washing/repair facilities. Failure to enforce the effective use of petrol / oil interceptors could result in oil-polluted discharges entering local watercourses.





**Offices**  
 Woking  
 London  
 Milton Keynes  
 Warsaw

**Architecture**  
 Planning  
 Master Planning  
 Urban Design  
 Interiors

24 Church St. West  
 Woking, Surrey,  
 GU21 6HT  
 01483 494 350

Revisions:  
 Rev A: Ground floor updated to current layout. OS amended at Hertford Road junction  
 Rev B: Ground floor & parking reconfigured

Drawn/Chkd:  
 MAC  
 MAC

Date:  
 021215  
 220416

Client:  
 ST MODWEN

Project:  
 EDMONTON GREEN SHOPPING CENTRE

Drawing Title:  
 BLOCK PLAN

Scale @ A3:  
 1:500

Checked by:  
 SC

Date:  
 September 2015

Job No:  
 10014

Stage:  
 PL

Drawing No:  
 100

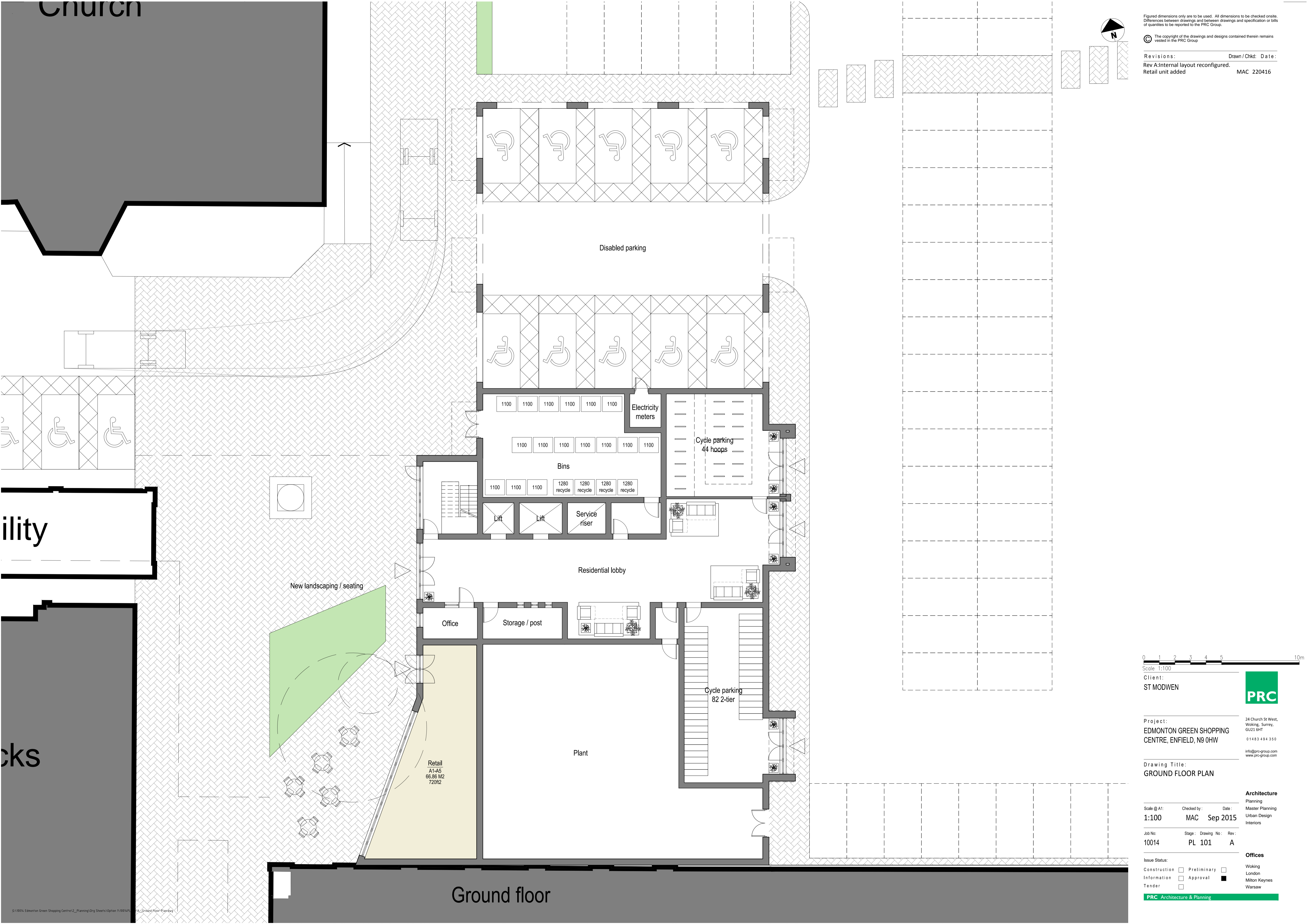
Rev:  
 B

Construction  Preliminary  Information   
 Approval  Tender



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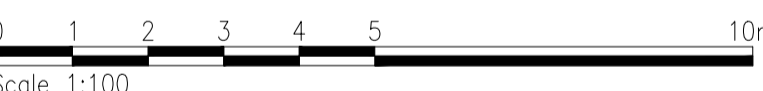
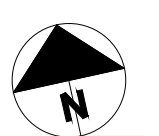




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Revisions: \_\_\_\_\_ Drawn / Chkd: \_\_\_\_\_ Date: \_\_\_\_\_  
 Rev A: Internal layout reconfigured. Retail unit added. MAC 220416



Scale 1:100  
 Client: ST MODWEN



Project: EDMONTON GREEN SHOPPING CENTRE, ENFIELD, N9 0HW

24 Church St West, Woking, Surrey, GU24 6HT  
 01483 494 350  
 info@prc-group.com  
 www.prc-group.com

Drawing Title: GROUND FLOOR PLAN

Scale @ A1: 1:100  
 Checked by: MAC  
 Date: Sep 2015

Architecture: Planning, Master Planning, Urban Design, Interiors

Job No: 10014  
 Stage: PL  
 Drawing No: 101  
 Rev: A

Issue Status:  
 Construction  Preliminary   
 Information  Approval   
 Tender

Offices: Woking, London, Milton Keynes, Warsaw

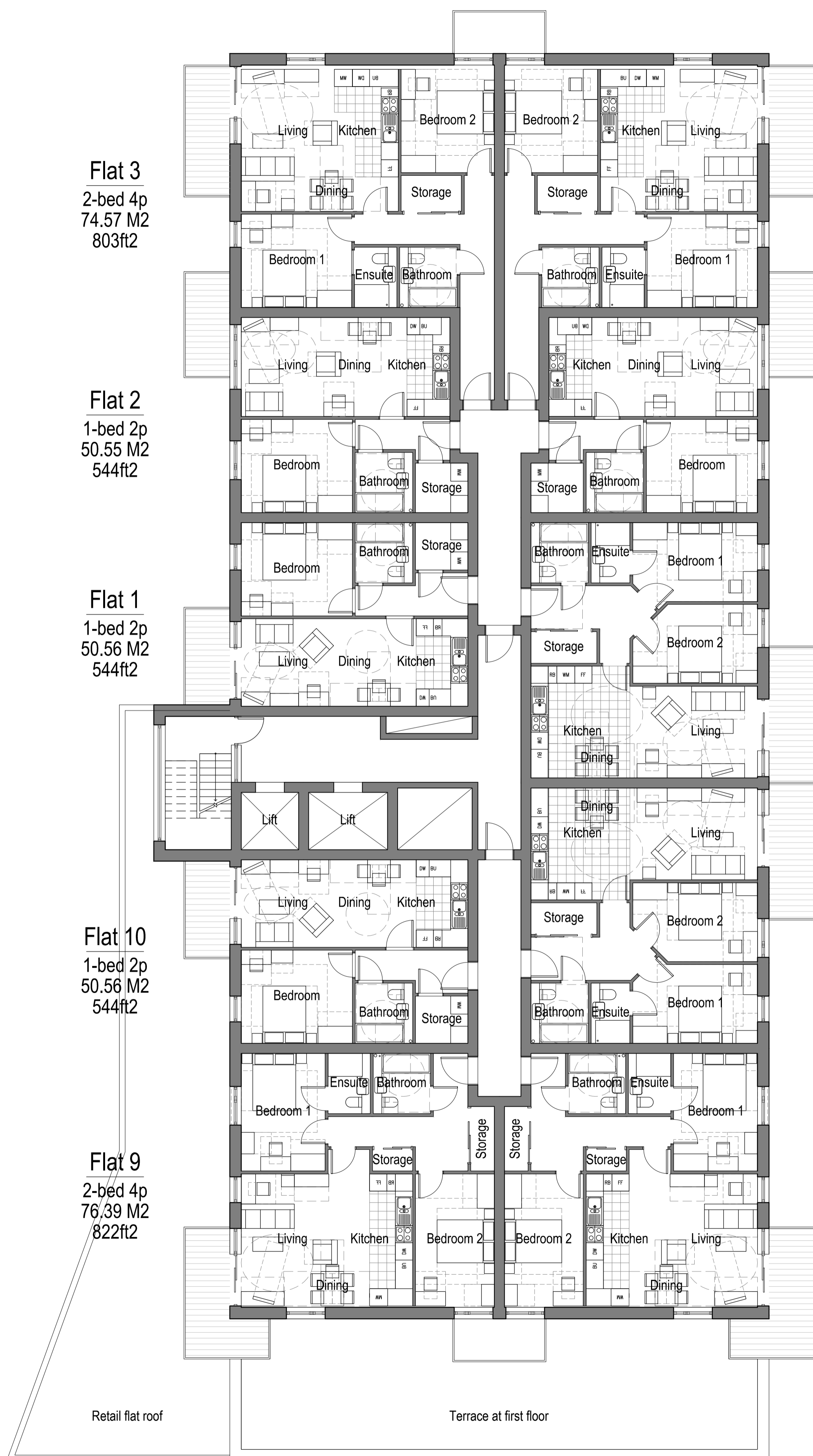




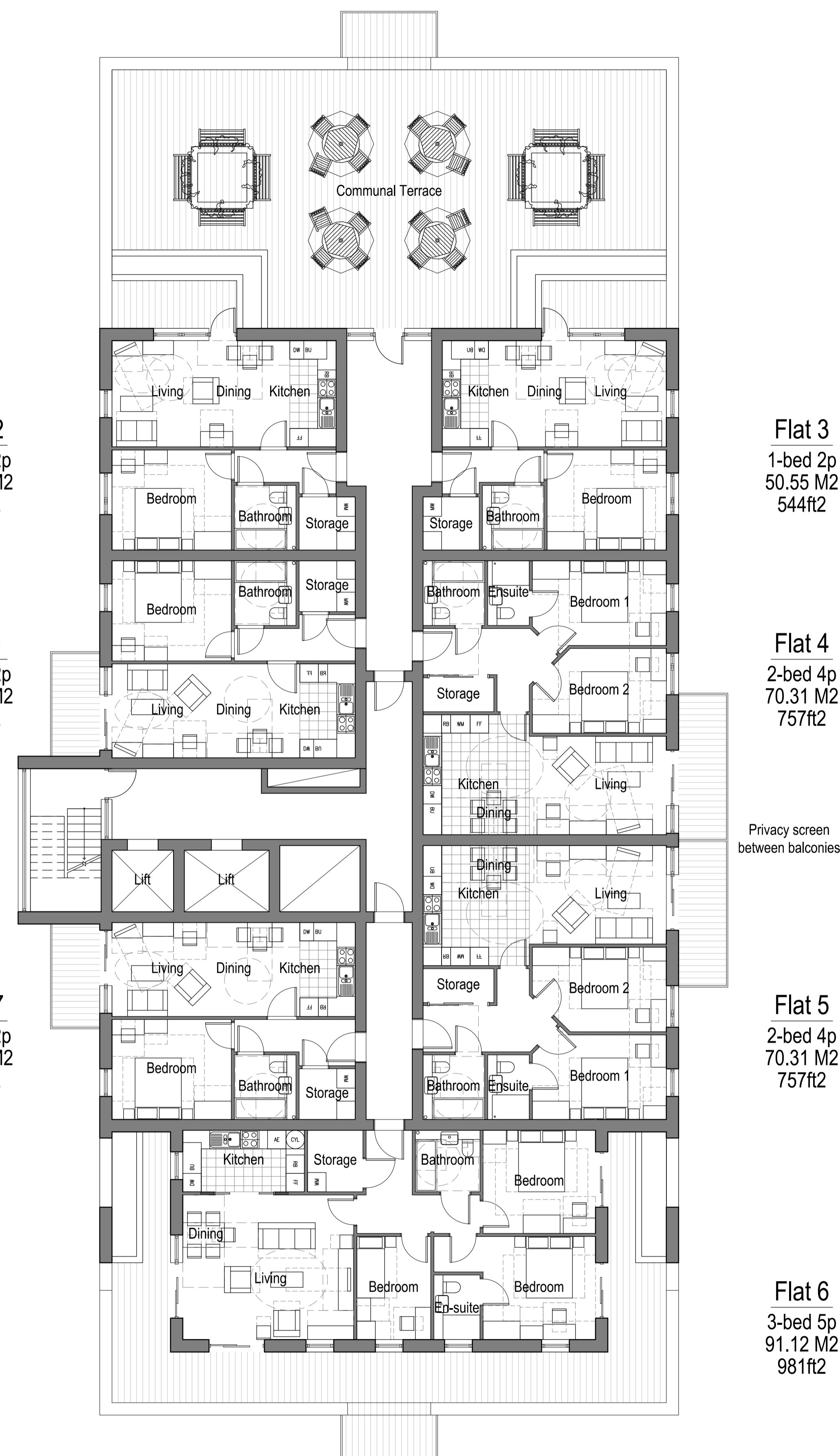
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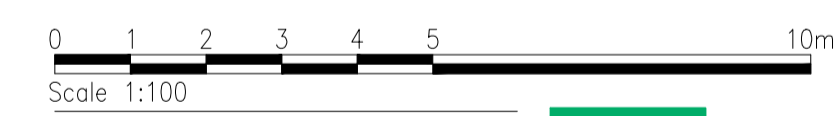
Revisions: Drawn / Chkd: Date:  
Rev A: Windows / balconies adjusted. MAC 220416  
Top floor plan reconfigured



1st - 7th floors



8th floor



Scale 1:100  
Client:  
ST MODWEN



Project:  
EDMONTON GREEN SHOPPING CENTRE, ENFIELD, N9 0HW

24 Church St West,  
Woking, Surrey,  
GU24 6HT  
01483 494 350  
info@prc-group.com  
www.prc-group.com

Drawing Title:  
FIRST TO EIGHTH FLOOR PLANS

Architecture  
Planning  
Master Planning  
Urban Design  
Interiors

Scale @ A1: 1:100  
Checked by: MAC  
Date: Sep 2015

Job No: 10014  
Stage: PL 102  
Drawing No: A

Issue Status:  
Construction  Preliminary   
Information  Approval   
Tender

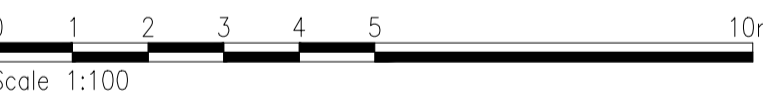
Offices  
Woking  
London  
Milton Keynes  
Warsaw

PRC Architecture & Planning





- Materials:**
- Roof** - Flat roof, single ply membrane
  - Aluminium fascia profile. Powder coated light grey
  - Walls** - Light grey multi stock brick
  - Aluminium cladding panels. Powder coated mid grey, green, white, & cream to match shopping centre panelling
  - Windows** - Aluminium frames. Powder coated light grey
  - Balconies** - Glass panels
  - Stainless steel handrails
  - Aluminium fascia panels. Powder coated white



Client:  
ST MODWEN



Project:  
EDMONTON GREEN SHOPPING CENTRE, ENFIELD, N9 0HW

24 Church St West,  
Woking, Surrey,  
GU21 6HT  
01483 494 350

info@prc-group.com  
www.prc-group.com

Drawing Title:  
WEST ELEVATION

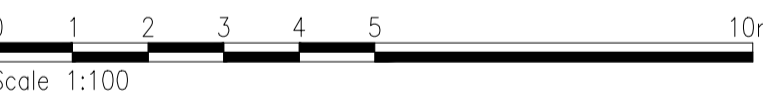
Scale @ A1: <b>1:100</b>	Checked by: <b>MAC</b>	Date: <b>Sep 2015</b>
Job No: <b>10014</b>	Stage: <b>PL 104</b>	Rev: <b>A</b>

- Issue Status:**
- Construction  Preliminary
  - Information  Approval
  - Tender
- Offices**
- Woking
  - London
  - Milton Keynes
  - Warsaw





- Materials:**
- Roof** - Flat roof, single ply membrane
  - Aluminium fascia profile. Powder coated light grey
  - Walls** - Light grey multi stock brick
  - Aluminium cladding panels. Powder coated mid grey, green, white, & cream to match shopping centre panelling
  - Windows** - Aluminium frames. Powder coated light grey
  - Balconies** - Glass panels
  - Stainless steel handrails
  - Aluminium fascia panels. Powder coated white



Scale 1:100  
 Client:  
**ST MODWEN**

**Project:**  
 EDMONTON GREEN SHOPPING CENTRE, ENFIELD, N9 0HW

24 Church St West,  
 Woking, Surrey,  
 GU21 6HT  
 01483 494 350  
 info@prc-group.com  
 www.prc-group.com

**PRC**

Drawing Title:  
**NORTH ELEVATION**

**Architecture**  
 Planning  
 Master Planning  
 Urban Design  
 Interiors

Scale @ A1: 1:100  
 Checked by: MAC  
 Date: Sep 2015

Job No: 10014  
 Stage: PL  
 Drawing No: 105  
 Rev: A

Issue Status:  
 Construction  Preliminary   
 Information  Approval   
 Tender

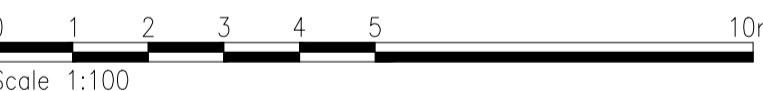
**Offices**  
 Woking  
 London  
 Milton Keynes  
 Warsaw

**PRC Architecture & Planning**





- Materials:**
- Roof** - Flat roof, single ply membrane
  - Aluminium fascia profile. Powder coated light grey
  - Walls** - Light grey multi stock brick
  - Aluminium cladding panels. Powder coated mid grey, green, white, & cream to match shopping centre panelling
  - Windows** - Aluminium frames. Powder coated light grey
  - Balconies** - Glass panels
  - Stainless steel handrails
  - Aluminium fascia panels. Powder coated white



Client:  
ST MODWEN



Project:  
EDMONTON GREEN SHOPPING CENTRE, ENFIELD, N9 0HW

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Drawing Title:  
EAST ELEVATION

**Architecture**  
 Planning  
 Master Planning  
 Urban Design  
 Interiors

Scale @ A1: 1:100  
 Checked by: MAC  
 Date: Sep 2015

Job No: 10014  
 Stage: PL  
 Drawing No: 106  
 Rev: A

**Offices**  
 Woking  
 London  
 Milton Keynes  
 Warsaw

PRC Architecture & Planning

Issue Status:  
 Construction  Preliminary   
 Information  Approval   
 Tender





wilko

PEACOCKS



10  
Shoppability



